

<u>55 2830</u> TOP = 244.48' LID LOCKED - NO ACCESS

<u>55 1570</u> TOP = 207.80' NV IN = 194.65' (10" FROM 2830) INV OUT = 194.60' (10" TO 2354)

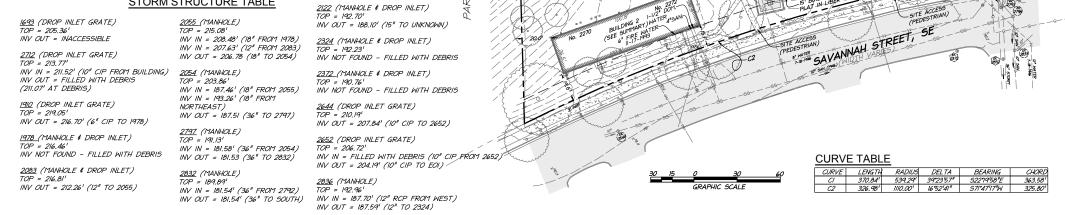
<u>55 2354</u> TOP = 192.04' INV IN = 179.04' (10" FROM 1570) INV OUT = 178.94' (10" TO 2798)

<u>SS 2544</u> TOP = 192.34' NV IN = 179.44' (10" FROM WEST) INV OUT = 179.39' (10" TO 2798)

INV IN = 177.52 (10" FROM 2544) INV OUT = 177.47' (10" TO 2833)

<u>55 2833</u> TOP = 190.27' INV IN = 177.47' (10" FROM EAST) INV IN = 177 02' (10" FROM 2798) INV OUT = 177.04' (10" TO SOUTH)

STORM STRUCTURE TABLE







COVER SHEET

AREA = ±4,427 SQUARE FEET (SEE GENERAL NOTE 8) HEIGHT = ±26.2'

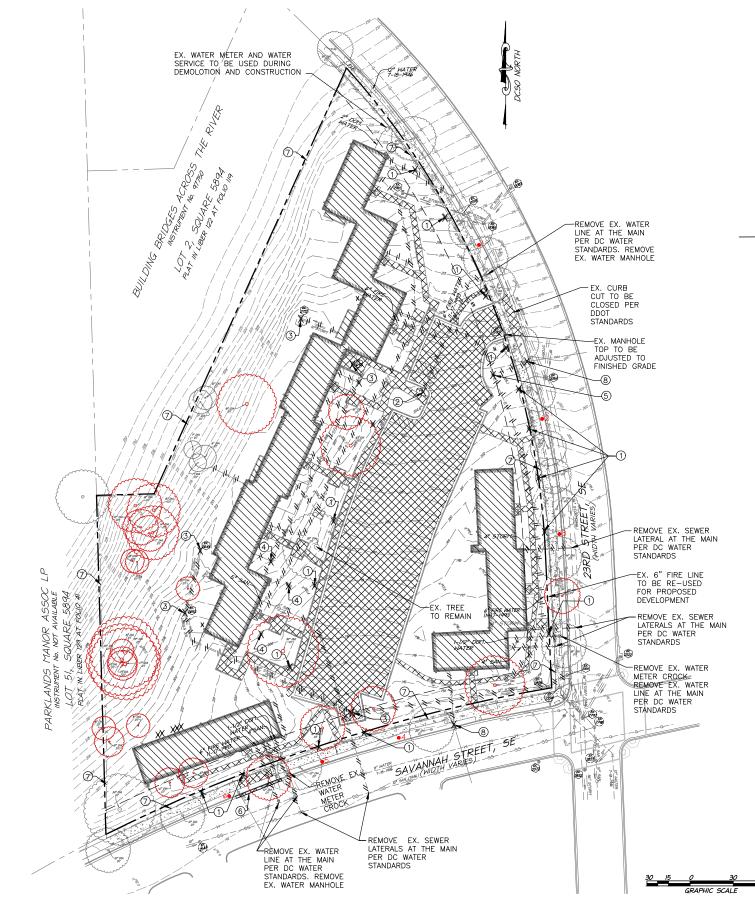


DEMOLITION NOTES

- CONTRACTOR SHALL COORDINATE WITH UTILITY COMPANIES FOR SHUTOFF, CAPPING, AND CONTINUATION OF UTILITY SERVICES AS REQUIRED. 1.
- CONTRACTOR SHALL REMOVE AND TRANSPORT ALL DEBRIS, RUBBISH, AND OTHER MATERIALS RESULTING FROM ALL DEMOLITION OPERATIONS TO A LEGAL OFF-SITE DISPOSAL FACILITY. 2.
- REMOVAL OF ASPHALT AND CONCRETE PAVEMENT SHALL INCLUDE THE REMOVAL OF ALL SURFACE, BASE AND SUBBASE MATERIALS.
- 4. CONTRACTOR SHALL VERIFY ALL EXISTING SITE CONDITIONS PRIOR TO BID AND CONSTRUCTION. ANY DISCREPANCIES IN THE PLANS SHALL BE BROUGHT TO THE ATTENTION OF THE OWNER'S REPRESENTATIVE IN WRITING, PRIOR TO FINAL BID AND STARTING ANY WORK.
- CONTRACTOR SHALL VERIFY THE ELEVATIONS OF ALL EXISTING UTILITIES WELL IN ADVANCE OF CONDUCTING CONSTRUCTION OPERATIONS TO ENSURE PROPER CONNECTIONS WITH PROPOSED UTILITIES.
- 6. PROPOSED WATER AND SEWER UTILITY WORK WITHIN THE PUBLIC RIGHT-OF-WAY TO BE PERFORMED UNDER INSPECTION OF DC WATER.
- ALL UNDERGROUND UTILITY LOCATIONS, INCLUDING WATER, STORM DRAINAGE, SANITARY SEWER, ELECTRICAL, TELEPHONE AND GAS WERE TAKEN FROM AVAILABLE RECORDS AND FIELD VERIFIED WHERE POSSIBLE. THE LOCATION OF ALL UTILITIES SHOWN ARE APPROXIMATE. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY AND DETERMINE THE EXACT LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO COMMENCING WORK. REPORT AND DISCREPANCY TO THE ENGINEER. CONTRACT MISS UTILITY' AT 1–800–257–7777, AT LEAST 48 HOURS PRIOR TO ANY EXCAVATION.
- CONTRACTOR MUST HAND DIG TEST PITS AT ALL UTILITY CROSSINGS TO DETERMINE THE EXACT LOCATION AND DEPTH OF ALL UTILITIES WELL IN ADVANCE OF DEMOLITION WORK AND PRIOR TO ORDERING PIPE MATERIALS AND STRUCTURES. UTILITIES FOUND DURING DEMOLITION OR CONSTRUCTION ACTIVITIES SHALL BE THE SOLE RESPONSIBILITY OF ANY CONTRACTOR ENGAGED IN EXCAVATION AT THIS SITE. THE DEMOLITION OF DATA CONTRACTOR ENGAGED IN EXCAVATION AT THIS SITE. THEN DEMOLITION OF DATA CONTRACTOR ENGAGED IN EXCAVATION AT THIS SITE. THEN DEMOLITION OF DATA CONTRACTOR ENGAGED IN EXCAVATION AT THIS SITE. THEN DEMOLITION OF DATA CONTRACTOR ENGAGED IN EXCAVATION AT THIS SITE. THEN DEMOLITION CONTRACTOR ENGAGED IN EXCAVATION AT THIS SITE. THEN 8. OWNER'S REPRESENTATIVE SHALL BE NOTIFIED IMMEDIATELY OF ANY UTILITY FINDINGS WHICH DEVIATE FROM THE CONDITIONS SHOWN.
- ALL SIDEWALKS, CURBS, CURBS & GUTTER AND DRIVEWAYS DISTURBED OR DAMAGED DURING CONSTRUCTION SHALL BE REPLACED IN COMPLETE SECTIONS TO THE NEAREST JOINT PER DDOT STANDARDS AND SPECIFICATIONS AT NO ADDITIONAL COST.
- NOTE PROXIMITY OF ADJACENT STRUCTURES AND UTILITY LINES AND MAINTAINED CONTINUED SERVICE DURING CONSTRUCTION. COORDINATE WITH RESPECTIVE UTILITY COMPANIES AND ENGINEER SHOULD THE RELOCATION OF SERVICE(S) BE REQUIRED.
- 11. EXISTING UTILITIES (STRUCTURES AND LINES) NOT REQUIRED FOR FUTURE SERVICE TO BE REMOVED TO FACILITATE CONSTRUCTION. UTILITIES TO BE CAPPED AS PER UTILITY PURVEYOR'S STANDARDS AND SPECIFICATIONS. COORDINATE REQUIREMENTS WITH UTILITY PURVEYOR'S. THIS INCLUDES EXISTING TRANSFORMERS AND CONCRETE TRANSFORMER PADS.
- REMOVAL OF ALL WALLS/RETAINING WALLS AND FENCES SHALL INCLUDE THE REMOVAL OF THEIR FOUNDATION UNLESS OTHERWISE INDICATED ON DRAWINGS. ALL EXISTING ON-SITE FENCING IS TO BE REMOVED.
- ALL EXISTING DC STREETLIGHT POLES THAT ARE BEING PERMANENTLY REMOVED MUST BE RETURNED IN GOOD CONDITION TO THE DISTRICT OF COLUMBIA WAREHOUSE AT 1735 15TH STREET, NE OFF OF WEST VIRGINIA AVENUE. CONTACT NUMBER 202-576-5258.
- 14. IN AREAS WHERE PROPOSED CONSTRUCTION MAY CONFLICT WITH EXISTING UTILITIES, THE CONTRACTOR SHALL TAKE NECESSARY PRECAUTIONS TO AVOID DAMAGE TO EXISTING UTILITIES. IF ANY UNDERGROUND UTILITY IS DAMAGED, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER'S REPRESENTATIVE AND THE OWNER OF THE
- 15. ANY DAMAGE SUSTAINED TO UTILITIES ABOVE AND BELOW GROUND SHALL BE REPAIRED BY OR UNDER THE DIRECTION OF THE UTILITY OWNER AT THE CONTRACTOR'S EXPENSE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BACKFILL OR EXCAVATE THE AFFECTED UTILITY WITHOUT FIRST RECEIVING PERMISSION FROM THE UTILITY OWNER. BACKFILL REMOVED FROM TRENCHES MAY ONLY BE REUSED IF IT COMPLIES WITH THE FILL REQUIREMENTS LISTED IN THE SPECIFICATIONS.
- 16. EXISTING WATER AND SEWER SERVICES NOT REQUIRED FOR FUTURE USE ARE TO BE REMOVED TO EXTENT NECESSARY TO FACILITATE NEW CONSTRUCTION. REMAINDER OF SERVICE TO BE CAPPED AT MAIN AND EXISTING VALVES AND TEES TO BE REMOVED PER DC WATER STANDARDS AND SPECIFICATIONS. COORDINATE REQUIREMENTS WITH DC WATER UTILITY INSPECTOR AT 202-787-4299. PAVEMENT TO BE REMOVED PER DDOT STANDARDS AND SPECIFICATIONS.
- ABANDONMENT OF EXISTING 2" AND SMALLER DIAMETER WATER SERVICES:
 ASSURE THAT FINAL METER READING HAS BEEN OBTAINED. RETURN DC WATER OWNED WATER METER WITH AMR READING, TRANSMITTER
- (IF ANY) TO DC WATER CUSTOMER SERVICE METER BRANCH. 2) EXCAVATE AND EXPOSE THE EXISTING TAP, AND CAP THE CONNECTION AT PUBLIC WATER MAIN. THE REMAINING WATER SERVICE PIPE IS ABANDONED IN PLACE.
- WHEN METER PIT IS LOCATED IN PUBLIC SPACE, REMOVE FRAME AND COVER, AND BACKFILL IT.
- A MINIMUM OF THE TOP TWO (2) FEET OF ALL EXISTING VALVE BOXES DESIGNATED TO BE ABANDONED, INCLUDING THE FRAME AND COVER, SHALL BE REMOVED. THE REMAINING PORTION OF THE STRUCTURE SHALL BE FILLED WITH NON COMPACTABLE SELECT STATUS DARFILL. NON-COMPACTABLE SELECT STONE BACKFILL.
- 18. UTILITY ABANDONMENT WORK WILL BE COMPLETED DURING THE RAZE PROCESS.
- 19. CONTRACTOR TO BE RESPONSIBLE FOR LAYOUT, EXTENT AND DESIGN OF SHEETING, SHORING AND SUPPORT OF EXISTING UTILITIES AND ADJACENT STRUCTURES. SHORING, BRACING AND UNDERPINNING SHALL BE DESIGNED BY A STRUCTURAL ENGINEER, LICENSED IN THE DISTRICT OF COLUMBIA, HIRED BY THE CONTRACTOR AS NECESSARY TO ENSURE SUPPORT OF SURROUNDING STRUCTURES AND UTILITIES.
- 24. EXISTING STREET LIGHT POLE RELOCATIONS WILL REQUIRE NEW POWER LINE CONNECTIONS INTO EXISTING ELECTRICAL CONDUITS OR MANHOLES, WHICH MAY REQUIRE ADDITIONAL PAVEMENT REMOVAL AND REPLACEMENT.
- 25. CONTACT DDOT-PUBLIC SPACE MAINTENANCE ADMINISTRATION 48 HOURS PRIOR TO START OF CONSTRUCTION AT 202-645-6030 OR 202-645-6031
- 26. FOR ALL EXISTING PIPES THAT ARE SHOWN TO BE REMOVED, IF ANY PORTION OF THE EXISTING PIPE IS LEFT IN THE GROUND, IT MUST BE GROUTED FULL.
- 27. CONTRACTOR TO PROVIDE TREE PROTECTION PER DDOT'S, URBAN FORESTRY ADMINISTRATION STANDARDS FOR ALL EXISTING TREES THAT ARE TO REMAIN WITHIN PUBLIC SPACE.







TERRACE MANOR 3301 23RD ST SE REDEVELOPMENT - PUD SUPPLEMENTAL SUBMISSION | 16 DECEMBER 2019

EXISTING CONDITIONS / DEMOLITION PLAN

DEMOLITION LEGEND

EX. UTILITY LINE TO BE REMOVED EX. MISCELLANEOUS SITE FEATURE OR STRUCTURE TO BE REMOVED EX. PAVEMENT TO BE REMOVED ALONG WITH ASSOCIATED CURB & GUTTER, STEPS & RETAINING WALLS EX. BUILDING TO BE REMOVED, INCLUDING ALL ASSOCIATED ITEMS

PROPERTY LINES

EX. TREE TO BE REMOVED

SIX RANDOM PIECES OF ~24" RCP STICKING UP OUT OF THE GROUND. REMOVE THEM. IF THE PIPE KEEPS GOING THEN CUT THEN OFF ~1' BELOW GRADE, GROUT, AND COVER THEM WITH DIRT AND GRASS SEED.

ADDITIONAL DEMOLITION CALL-OUTS

- REMOVE EX. SITE LIGHT REMOVE EX. FIRE HYDRANT REMOVE EX. STORM INLET OR STORM MANHOLE REMOVE EX. CLEANOUT REMOVE EX. CLEANOUT REMOVE EX. CURB & GUTTER REMOVE EX. CURB & GUTTER REMOVE EX. SITE FENCING



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PROJECT NARRATIVE

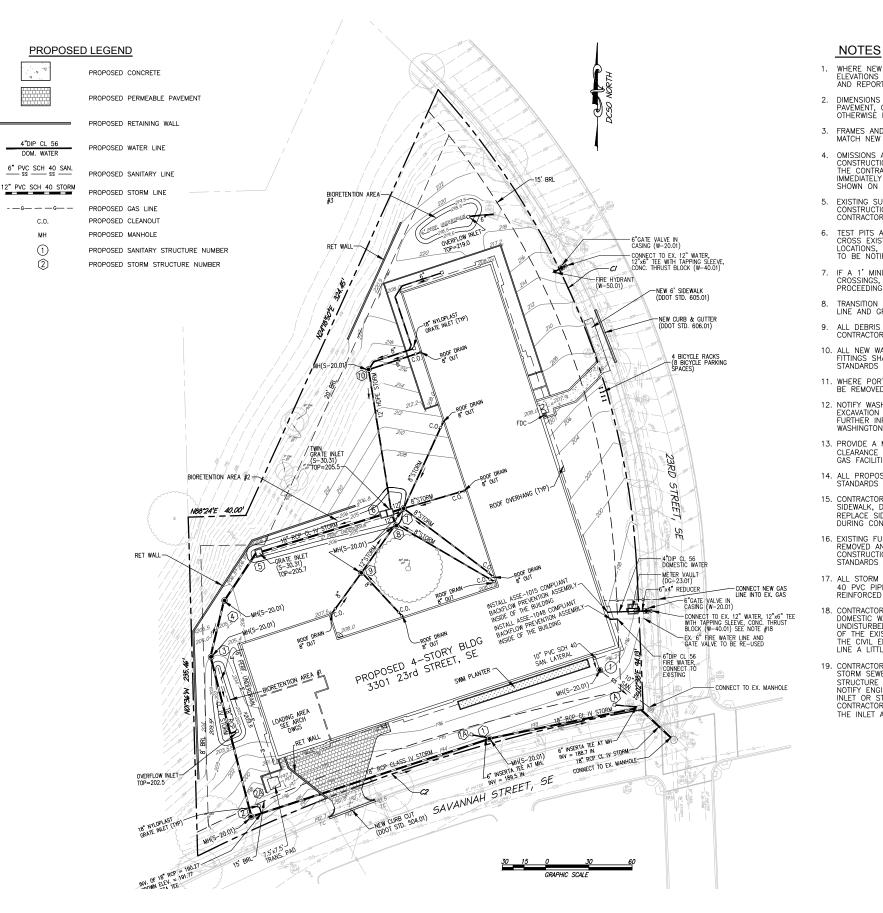
THE PROJECT IS LOCATED AT 3301 23RD STREET SE, WASHINGTON, DC. THE SITE PROPERTY IS COMPOSED OF THREE PARCELS OPERATING AS A RESIDENTIAL APARTMENT COMPLEX. THE EXISTING APARTMENTS BUILDINGS WILL BE RAZED, INCLUDING THE EXISTING PARKING AREA, AND A NEW 4-STORY APARTMENT BUILDING WITH UNDERGROUND PARKING WILL BE CONSTRUCTED. THERE ARE 130 RESIDENTIAL UNITS PROPOSED AS PART OF THIS PROJECT.

THE EXISTING BUILDING HAS EXISTING ELECTRIC, TELECOM, WATER AND SANITARY SEWER SERVICES. ALL OF THESE EXISTING SERVICES WILL BE ABANDONED OR REMOVED.

NEW UTILITIES WILL BE PROVIDED TO SERVICE THE PROPOSED APARTMENT BUILDING AS PART OF THIS PROJECT, WHICH INCLUDES A NEW DOMESTIC WATER LINE, A NEW FIRE WATER LINE, NEW STORM AND SANITARY LATERALS, AND NEW ELECTRIC SERVICE.

THE SITE DRAINS FROM THE NORTHWEST TO THE SOUTHEAST WITH EXISTING SLOPES MOSTLY IN THE RANGE OF 8 TO 15% ONSITE AND SOME ADJOINING SLOPES UP TO 40%. THE EXISTING SOILS ONSITE HAVE BEEN ALTERED BY PREVIOUS DEVELOPMENT AND PREDOMINATELY CONSIST OF KEPORT URBAN LAND COMPLEX AND UDORTHANTS. THE ADJOINING STEEP SLOPES CONSIST OF CHRISTIANA-URBAN LAND COMPLEX AND CROOM VERY GRAVELLY SANDY LOAM, ONSITE SOIL TESTING INDICATES PERCOLATION RATES THAT WILL REQUIRE SWM/BMP MEASURES TO HAVE UNDERDRAINS.

THERE ARE NO EXISTING STORMWATER MANAGEMENT (SWM) FACILITIES ON THE PROPERTY. PROPOSED SWM MEASURES AS PART OF THIS PROJECT INCLUDE GREEN ROOF, BIORETENTION FACILITIES AND PLANTING LARGE TREES.





SITE AND UTILITY PLAN

WHERE NEW WORK MEETS EXISTING, NOTE FIELD LOCATIONS AND ELEVATIONS OF EXISTING FEATURES BEFORE BEGINNING CONSTRUCTION AND REPORT ANY DISCREPANCIES TO THE ARCHITECT OR ENGINEER.

DIMENSIONS ARE TO FACE OF WALL AND CURB, EDGE OF WALK OR PAVEMENT, CENTERLINE OF PIPE OR UTILITY STRUCTURE, UNLESS OTHERWISE NOTED.

3. FRAMES AND COVERS OF EXISTING STRUCTURES TO BE ADJUSTED TO MATCH NEW FINISHED GRADES AS NEEDED.

4. OMISSIONS AND/OR ADDITIONS OF UTILITIES FOUND DURING CONSTRUCTION SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL NOTIFY THE ARCHITECT OR ENGINEER IMMEDIATELY IF ANY INFORMATION CONCERNING FOUND UTILITY IS NOT SHOWN ON PLANS.

EXISTING SURFACE CONDITIONS DISTURBED OR DAMAGED DURING CONSTRUCTION SHALL BE REPLACED TO MATCH EXISTING CONDITIONS. CONTRACTOR TO COORDINATE EXTENT WITH ARCHITECT OR ENGINEER.

6. TEST PITS ARE REQUIRED AT ALL LOCATIONS WHERE PROPOSED UTILITIES CROSS EXISTING UTILITIES. INVESTIGATIONS TO IDENTIFY HORIZONTAL LOCATIONS, ELEVATION AND SIZE OF EXISTING UTILITIES. THE ENGINEER IS TO BE NOTIFIED OF THIS INFORMATION.

IF A 1' MINIMUM VERTICAL CLEARANCE CANNOT BE MAINTAINED AT UTILITY CROSSINGS, THE CONTRACTOR IS TO NOTIFY THE ENGINEER BEFORE PROCEEDING WITH WORK.

TRANSITION CURB, GUTTER, PAVING AND SIDEWALK TO MEET EXISTING IN LINE AND GRADE OR AS DIRECTED BY ENGINEER.

9. ALL DEBRIS AND EXCESS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT AN APPROVED OFF-SITE LOCATION.

10. ALL NEW WATER LINES TO HAVE A MINIMUM COVER OF 4 FEET. WATER FITTINGS SHALL BE PROPERLY TIED AND ANCHORED PER DC WATER STANDARDS AND SPECIFICATIONS.

11. WHERE PORTIONS OF EXISTING BITUMINOUS OR CONCRETE PAVING ARE TO BE REMOVED, THE EXISTING PAVEMENT SHALL BE SAW-CUT.

NOTIFY WASHINGTON GAS AT 202-750-4205, 48 HOURS PRIOR TO AN EXCAVATION IN THE VICINITY OF ANY GAS TRANSMISSION MAIN. FOR FURTHER INFORMATION OR PROBLEMS, CONTACT CHUCK WHITLEY AT WASHINGTON GAS AT 703-750-4205.

PROVIDE A MINIMUM OF 5 FEET OF HORIZONTAL AND 1 FOOT VERTICAL CLEARANCE BETWEEN 12" DIAMETER AND SMALLER DISTRIBUTION EXISTING GAS FACILITIES AND PROPOSED FACILITIES.

14. ALL PROPOSED WORK TO BE CONSTRUCTED IN ACCORDANCE WITH LATEST STANDARDS AND SPECIFICATIONS OF DDOT AND DC WATER.

15. CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING SIDEWALK, DRIVEWAYS, CURB AND GUTTER THAT IS TO REMAIN OR TO REPLACE SIDEWALK, DRIVEWAYS, AND/OR CURB AND GUTTER DAMAGED DURING CONSTRUCTION.

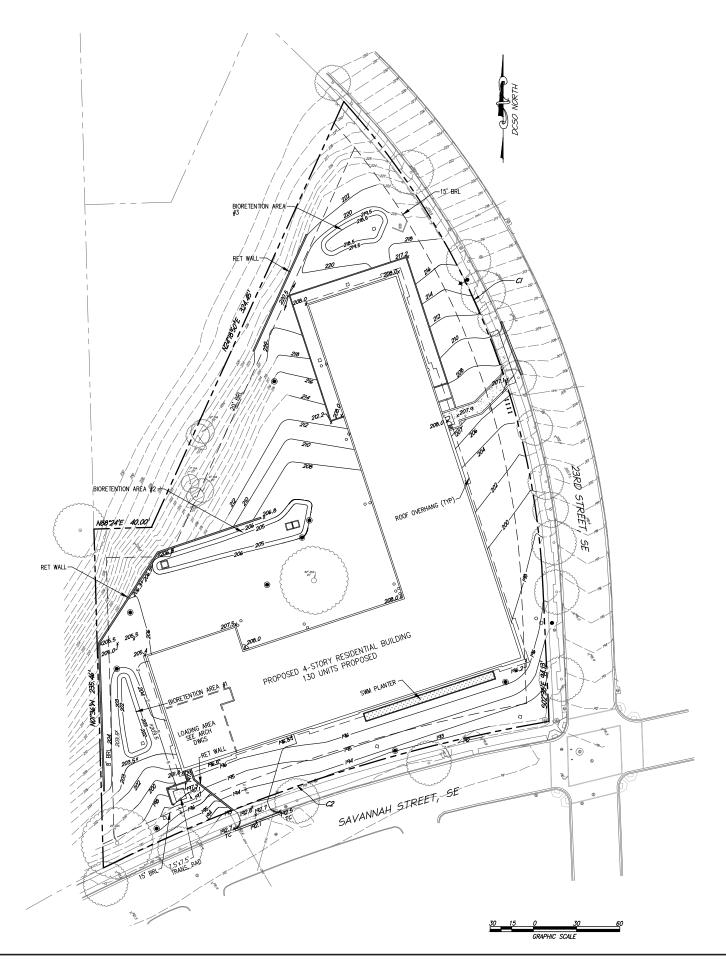
16. EXISTING FULL DEPTH PAVEMENT SECTION, CURB AND GUTTER TO BE REMOVED AND REPLACED TO EXTENT NECESSARY TO FACILITATE CONSTRUCTION OF NEW UTILITIES. MATERIALS TO COMPLY WITH DDOT STANDARDS AND SPECIFICATIONS.

ALL STORM DRAINS 12" IN DIAMETER OR SMALLER SHALL BE SCHEDULE 40 PVC PIPE. ALL STORM DRAINS OVER 12" IN DIAMETER SHALL BE REINFORCED CONCRETE PIPE, CLASS IV.

18. CONTRACTOR TO VERIFY IN THE FIELD THAT THE INSTALLATION OF DOMESTIC WATER LINE CONNECTION AT THE MAIN WILL NOT IMPACT THE UNDISTURBED SOIL BEHIND THE THRUST BLOCK AT THE TEE CONNECTION OF THE EXISTING FIRE WATER LINE THAT IS TO REMAIN. COORDINATE WITH THE CIVIL ENGINEER IF IT MAY BE NECESSARY TO SHIFT THE DOMESTIC LINE A LITTLE BIT FURTHER TO THE NORTH.

19. CONTRACTOR TO CLEAN THE EXISTING YARD INLET AND EXISTING 6" STORM SEWER OUTFALL PIPE ALL THE WAY OUT TO EXISTING STORM STRUCTURE #1978 LOCATED WITHIN PUBLIC SPACE. CONTRACTOR TO NOTIFY ENGINEER AND OWNER SHOULD THEY DISCOVER THAT EITHER THE INLET OR STORM PIPE ARE COMPROMISED AFTER CLEANING. IF SO, CONTRACTOR TO PROVIDE OWNER WITH AN ALTERNATE PRICE TO REPLACE THE INLET AND/OR STORM PIPE UP TO STRUCTURE #1978.

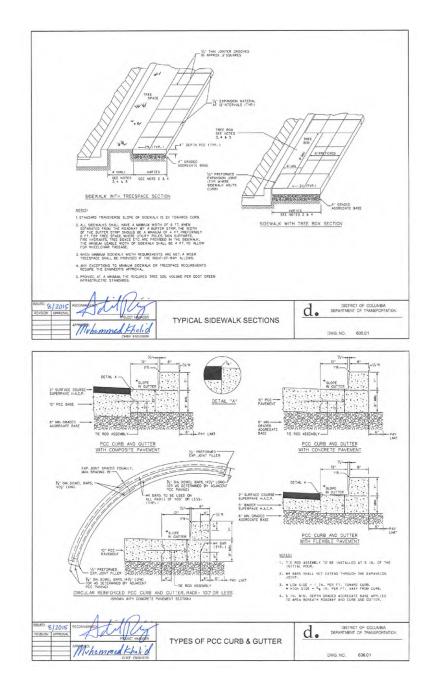


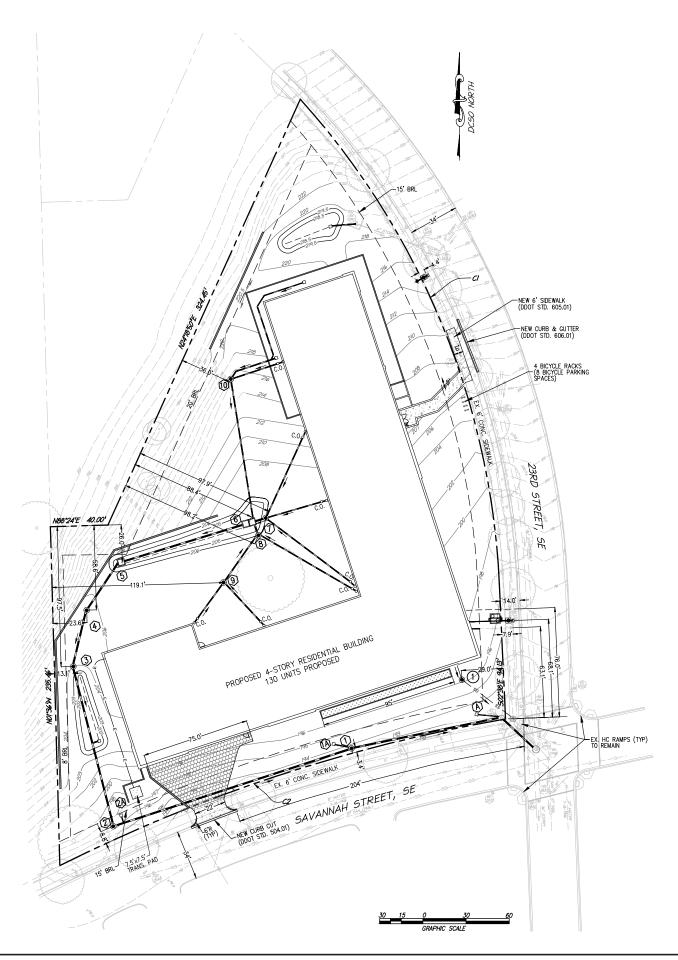




GRADING PLAN





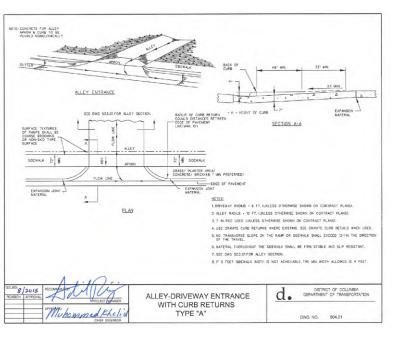






TERRACE MANOR 3301 23RD ST SE REDEVELOPMENT - PUD SUPPLEMENTAL SUBMISSION | 16 DECEMBER 2019

PUBLIC SPACE PLAN





STORMWATER MANAGEMENT NARRATIVE

THE EXISTING TERRACE MANOR APARTMENT BUILDINGS AND THE EXISTING PARKING LOT AREA ON THE PROPERTY WILL BE DEMOLISHED. THE PROJECT PROPOSES A NEW APARTMENT BUILDING AS WELL AS UNDERGROUND PARKING. THIS WORK IS CONSIDERED A MAJOR LAND DISTURBING ACTIVITY AND SHALL PROVIDE ON-SITE RETENTION OF THE FIRST 1.2 INCHES OF RAINFALL FOR THE ENTIRE AMOUNT OF DISTURBED AREA (93,470 SF). THESE NUMBERS WERE USED FOR THE STORMWATER MANAGEMENT COMPUTATIONS ALSO, THE PROPERTY IS ZONED RA-1 (FORMER R-5-A), THEREFORE, GREEN AREA REQUIREMENTS APPLY TO THIS PROJECT AND REQUIRES A MINIMUM GREEN AREA RATIO SCORE OF 0.40.

THE SITE IS LOCATED IN THE MS4 (SEPARATE SEWER SYSTEM AREA) AND IS WITHIN THE ANACOSTIA RIVER WATERSHED. HOWEVER, THE SITE IS LOCATED OUTSIDE OF THE ANACOSTIA WATERFRONT DEVELOPMENT ZONE (AWD2). THERE WILL BE A DECREASE IN STORM RUNOFF WHEN THE PROJECT IS CONSTRUCTED AND ALL STORWWATER MANAGEMENT MEASURES IMPLEMENTED WHEN COMPARED TO CURRENT EXISTING CONDITIONS.

THE GENERAL RETENTION COMPLIANCE CALCULATOR WAS USED IN DOEE'S SWM DATABASE TO DEMONSTRATE THE REQUIRED AMOUNT OF STORMWATER RETENTION HAS BEEN PROVIDED. THE SITE'S STORMWATER RETENTION VOLUME (SWRV) IS BASED UPON 93,470 SF OF "MAJOR LAND DISTURBING ACTIVITY" WHICH UTILIZES A 1.2" REGULATORY RAIN EVENT. THIS RESULTS IN A SWRV OF 4,654 CF (35,328 GALLONS) THAT MUST BE PROVIDED FOR ON-SITE.

PROPOSED SWM CONTROLS INCLUDE A LARGE AMOUNT OF EXTENSIVE GREEN ROOF, THREE BIORETENTION AREAS, LARGE TREE PLANTINGS, PERMABLE PAVEMENT TO TREAT VEHICULAR TRAFFIC AND A STORWATER MANAGEMENT PLANTER ALONG THE SOUTH SIDE OF THE BUILDING.

NOTES

LEGEND

LOD

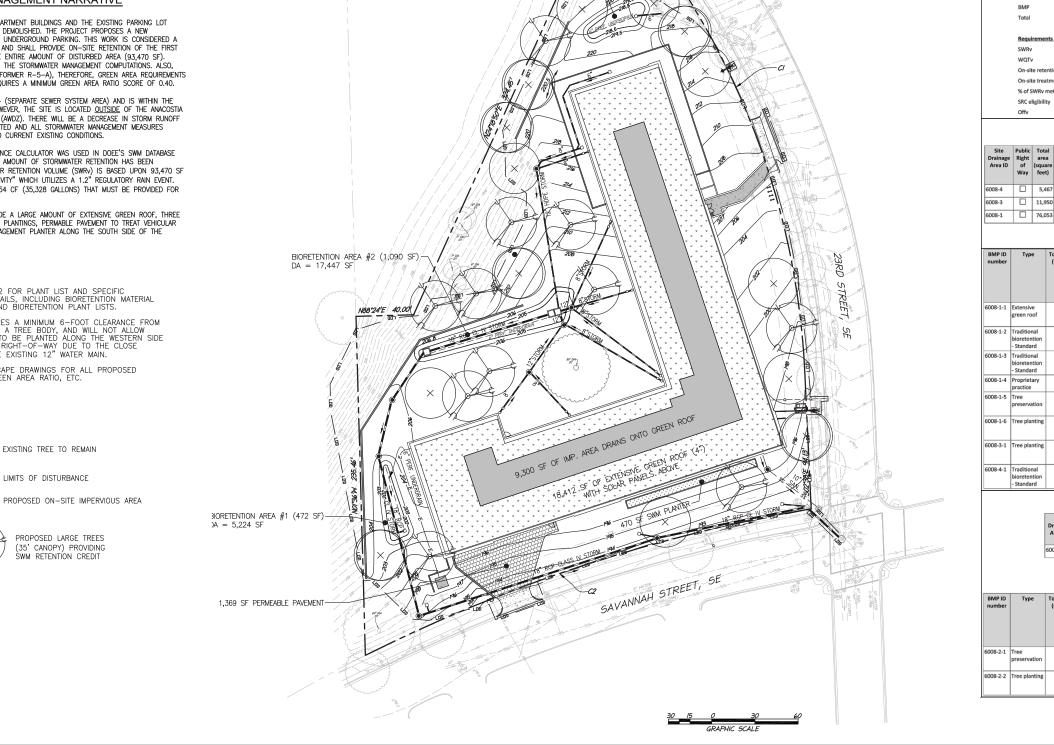
- SEE SHEET C-402 FOR PLANT LIST AND SPECIFIC BIORETENTION DETAILS, INCLUDING BIORETENTION MATERIAL SPECIFICATIONS AND BIORETENTION PLANT LISTS.
- 2. DC WATER REQUIRES A MINIMUM 6-FOOT CLEARANCE FROM A WATER MAIN TO A TREE BODY, AND WILL NOT ALLOW ANY NEW TREES TO BE PLANTED ALONG THE WESTERN SIDE OF 23RD STREET RIGHT-OF-WAY DUE TO THE CLOSE PROXIMITY OF THE EXISTING 12" WATER MAIN.

EXISTING TREE TO REMAIN

LIMITS OF DISTURBANCE

PROPOSED LARGE TREES (35' CANOPY) PROVIDING SWM RETENTION CREDIT

3. REFER TO LANDSCAPE DRAWINGS FOR ALL PROPOSED LANDSCAPING, GREEN AREA RATIO, ETC.



BIORETENTIÓN AREA #3 (474 SF) -DA = 5,467 SF





			St	ormwate	r Mana	agemer	nt Pla	n Co	mplia	nce Da	ta			
Site A	ddress			3301 23rd Str	eet SE	ı	lan nun	nber	60	08				
Storm	water M	anagemer	t Plan?	Yes		(Green Ar	ea Ratio	o? Ye	s				
Soil Er	osion an	d Sedimer	t Control?	Yes		F	loodpla	in Revie	w? No	,				
Туре о	of Activity	/		Major Land Di	sturbing	,	WDZ?		Non-AWD	z				
ls the	entire sit	e in the C	SS?	No										
		Total Are	ea (sf) Site	Area PR	ow		Curve Ni	umbers						
Natur	al	0	0	0					etention P	rovided				
Comp	acted	60,738	60,3	363 37	5		Pre-deve				ar storm adj	usted CN	66	
Imper	vious	13,284	12,6	559 62	5	1	Pre-proje	ect	92	15-y	ear storm ad	justed CN	72	
BMP		20,448	20,4	148 0						100-	year storm a	djusted CN	75	
Total		94,470	93,4	170 1,0	00									
Derent		c	(4-4-1 - 4)			- 0			63 D-		T-+-1 (4)	. T.I.		
SWRv		Summary	(total is th	ne sum of PRO	w and Parc	ei)		PROW (1 59		rcel (ft ³) 554	Total (ft ³ 4,723	35,3	l (Gallons)	
WQTv									4,0	554	4,723	35,3 0	28	
		on achieve						, 70		322	4,892	36,5	06	
		ent achiev					, (40		4,892	3,03		
	WRv me		eu					, 102%		4%	103.59%		2 59%	
	igibility	con-site						10270	10	470	200.0070	1,26		
Offv	-9- <i>-</i>											0	•	
				Si	te Drain	age Area	Comp	liance	Data					
							p		D ata					
Public Right of Way	Total area (square feet)	Natural (square feet)	Compacte (square feet)	d Impervious (square feet)	BMP (square feet)	Vehicular access area	SWRv (cubic feet)	WQTv (cubic feet)	Volume retained (cubic feet)	Volume treated (cubic feet)	2-year storm adjusted Curve Number	15-year storm adjusted Curve Number	100-year storm adjusted Curve Number	SDA Minimum Compliance
	5.467		4.96	13	474		170		241		40	58		Ves

320

4,422

2,845 4,165

11,950

11.650

43,720

300

12,359 19,974

				Site I	BMP Co	mpliance	e Data						
Гуре	Total CDA (square feet)	Natural (square feet)	Compacted (square feet)	Impervious (square feet)	BMP (square feet)	Total Post project vehicular access area	Volume received from upstream BMPs (cubic feet)	Max volume received by BMP (cubic feet)	Storage volume (cubic feet)	Retention calculation	Volume retained (cubic feet)	Volume treated (cubic feet)	Downstream BMP ID Numbers
nsive n roof	27,716			9,304	18,412			3,730	3,304	100% of storage volume	3,304		
tional tention ndard	5,224		4,752		472			232	685	60% of storage volume	232		
tional tention ndard	17,447		16,357		1,090			726	1,576	60% of storage volume	726		
rietary ice	3,480		635	2,845		2,845		405	699			405	
rvation										20 cubic feet per tree	20		
planting										10 cubic feet per tree	140		
planting										10 cubic feet per tree	160		
tional tention ndard	5,467		4,993		474			241		60% of storage volume	241		

PROW	Drainage	Area	Compl	iance	Data	

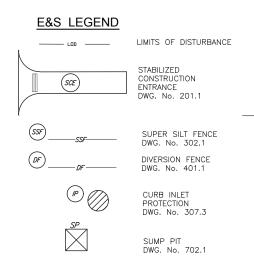
Site Drainage Area ID		Total area (square feet)	(square		Impervious (square feet)	BMP (square feet)	Vehicular access area	(cubic	retained	
6008-2	\boxtimes	1,000		375	625			69	70	

PROW BMP Compliance Data

Гуре	Total CDA (square feet)	Natural (square feet)	Compacted (square feet)	BMP (square feet)	Total Post project vehicular access area	Volume received from upstream BMPs (cubic feet)	volume	Retention calculation		Downstream BMP ID Numbers
ervation								20 cubic feet per tree	40	
planting								10 cubic feet per tree	30	



73 Yes



VΝ

NOTE: WHEN EXCAVATION IS PROPOSED IMMEDIATELY ADJACENT TO THE CRZ, ROOTS MUST FIRST BE PRUNED AT THE EDGE OF THE EXCAVATION WITH A TRENCHING MACHINE, VIBRATORY KNIFE OR VERTICAL SAW TO A DEPTH OF 18 INCHES.

<u>EROSION & SEDIMENT CONTROL NARRATIVE</u>

- THE CONTRACTOR SHALL CALL THE DOEE INSPECTIONS & ENFORCEMENT BRANCH, WATERSHED 1. PROTECTION DIVISION, AT (202) 535–2977 FOR A PRE-CONSTRUCTION MEETING 72 HOURS PRIOR TO THE START OF ANY LAND DISTURBING ACTIVITY.
- INSTALL PERIMETER SEDIMENT CONTROL MEASURES, SILT FENCE, TEMPORARY CONSTRUCTION 2. ENTRANCE AND INLET PROTECTIONS AS SHOWN ON THE EROSION AND SEDIMENT CONTROL PLAN. INSTALL SEDIMENT TANK AND SUMP PIT. RELOCATE AS NECESSARY TO EFFECTIVELY TREAT DIRTY WATER FROM LEAVING THE SITE DURING CONSTRUCTION.
- DEMOLISH EXISTING BUILDING, PAVEMENT, STAIRS, SIDEWALK, RETAINING WALL, UTILITIES, UTILITY STRUCTURES AND OTHER ITEMS AS SHOWN ON THE DEMOLITION PLAN. ABANDON EXISTING DOMESTIC WATER LINE AT THE MAIN PER DC WATER STANDARDS. .3.
- STABILIZE DENUDED AREAS. 4
- INSTALL ALL PROPOSED NEW UNDERGROUND UTILITIES AND UTILITY STRUCTURES.
- CONSTRUCT NEW BUILDING, RETAINING WALLS, DRIVEWAY ENTRANCE, AND ALL OTHER NEW WORK SHOWN ON THE SITE PLAN. BIORETENTION AREAS SHALL BE PROTECTED VIA SILT FENCE UNTIL SITE IS FULLY STABILIZED AND FINAL INSPECTION PERFORMED BY DOEE SITE INSPECTOR JUST PRIOR TO BUILDING OCCUPANCY.
- 7. REMOVAL OF EROSION AND SEDIMENT CONTROL MEASURES REQUIRES PRIOR APPROVAL FROM DOEE SITE INSPECTOR.

MAINTENANCE PROGRAM

ALL SEDIMENT CONTROL MEASURES ARE TO BE INSPECTED DAILY BY THE SITE SUPERINTENDENT AND ANY DAMAGED FACILITY IS TO BE REPAIRED BY THE CLOSE OF THE WORKDAY. TRAPS AT STORM STRUCTURES ARE TO BE CLEANED AFTER EACH RAINFALL AND INLET PROTECTION MAY BE REMOVED ONLY AFTER UPSTREAM AREAS HAVE BEEN STABILIZED WITH A PERMANENT SURFACE AND DOEE INSPECTOR APPROVAL. PROVIDE PORTBALE SEDIMENT TANK AND SUMP PIT IF REQUIRED TO DEWATER THE SITE.

EROSION AND SEDIMENT CONTROL MEASURES

THE FOLLOWING EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH CITY STANDARDS:

- PROVIDE SUPER SILT FENCE IN LOCATIONS SHOWN ON THE EROSION AND SEDIMENT CONTROL PLAN. INSTALL SAFETY FENCE AS NEEDED ALONG THE PERIMETER OF THE LIMITS OF DISTURBANCE.
- <u>STORM DRAIN INLET PROTECTION</u> SHALL BE PROVIDED FOR ALL EXISTING INLETS IN THE VICINITY OF THE PROPERTY THAT ARE TO REMAIN AS SHOWN ON THE SEDIMENT CONTROL PLAN.
- <u>CONSTRUCTION ENTRANCE</u> A TEMPORARY, STABILIZED CONSTRUCTION ENTRANCE IN ACCORDANCE WITH CITY STANDARDS WITH A LENGTH OF AT LEAST 50 LINEAR FEET WILL BE PROVIDED AT THE LOCATION SHOWN ON THE SEDIMENT CONTROL PLAN PLANS. THE ENTRANCE SHALL BE MAINTAINED IN GOOD REPAIR AND SHALL PROVIDED REMOVAL OF DEBRIS FROM VEHICLES PRIOR TO LEAVING THE CONSTRUCTION SITE. WATER FOR THE WASH RACK TO BE PROVIDED BY A WATER TANK TRUCK IF PUBLIC WATER IS NOT AVAILABLE. .3
- <u>DUST CONTROL</u> DUST CONTROL SHALL BE PROVIDED AS NECESSARY DURING DEMOLITION OPERATIONS TO PREVENT SURFACE AND AIR MOVEMENT OF DUST FROM EXPOSED SOIL SURFACES AND REDUCE THE PRESENCE OF AIRBORNE SUBSTANCES THAT MAY PRODUCE HEALTH HAZARDS OR TRAFFIC SAFETY PROBLEMS.
- IF AT ANY TIME DURING SITE DEMOLITION THERE IS ANY EXPOSED SOIL, IT SHALL BE 5. <u>STABILIZED WITH TEMPORARY SEEDING.</u> SEEDING MIXTURES AND SURFACE TREATMENT SHALL BE BASED UPON RECOMMENDATIONS LISTED ON SHEET C-801 AND SHALL BE VERIFIED WITH THE CITY INSPECTOR BASED UPON CONSTRUCTION DATES.

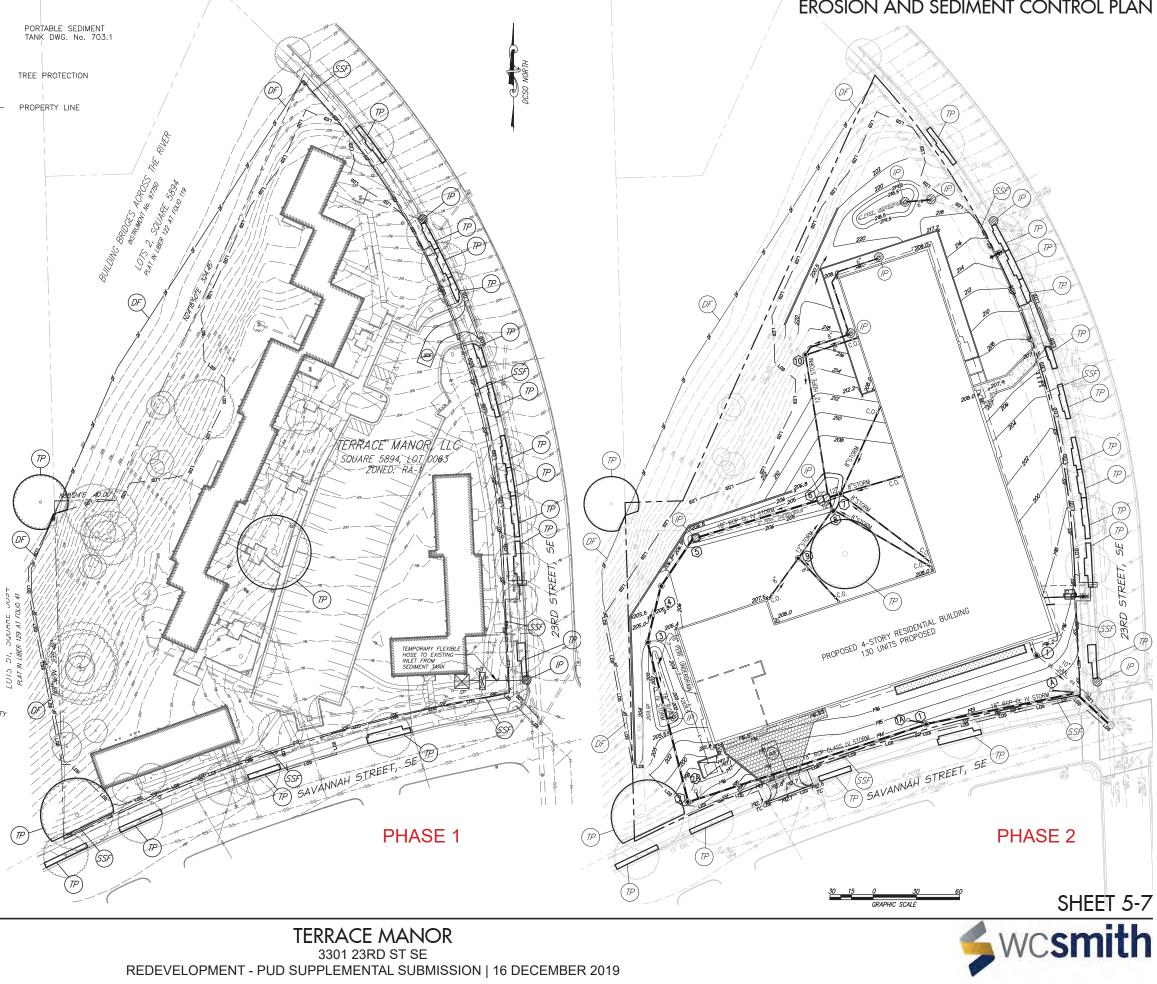
UTILITY INSTALLATION NOTES

- PLACE EXCAVATED MATERIALS UPSI OPF OF THE TRENCH.
- FILTER WATER PUMPED FROM EXCAVATIONS PRIOR TO DISCHARGING TO THE STORM SEWER SYSTEM.
- PROVIDE STABILIZATION (INTERIM OR PERMANENT) AFTER THE TRENCH IS REFILLED.

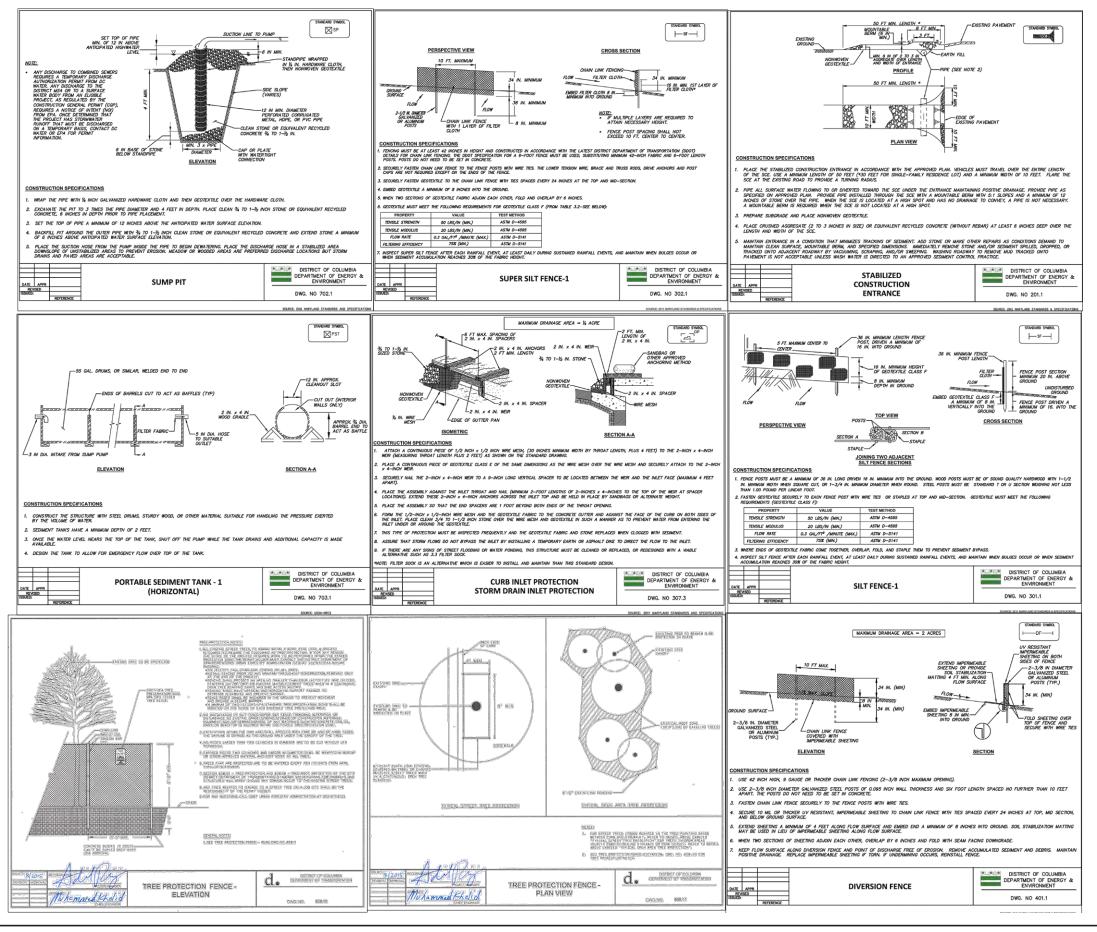
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EROSION AND SEDIMENT CONTROL PLAN



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EROSION AND SEDIMENT CONTROL DETAILS

